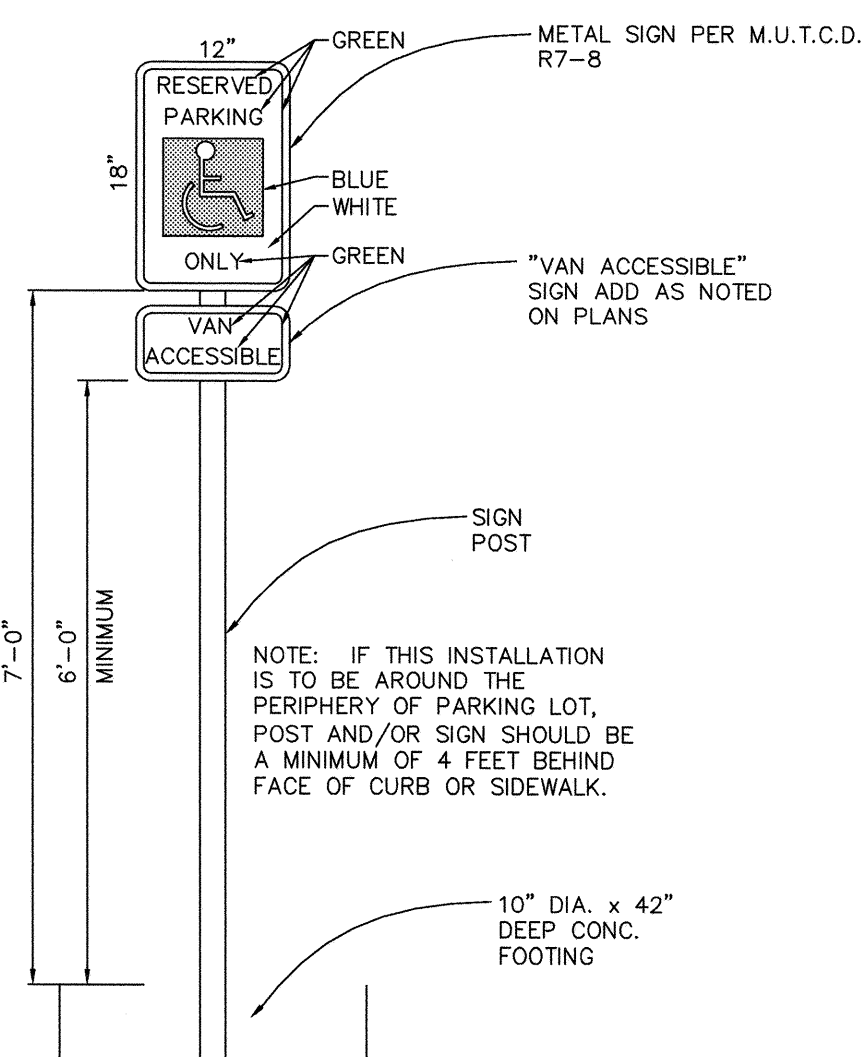


STEP ONE STEP TWO

- NOTES:
- CURB AND GUTTER SECTION SHALL BE REMOVED IN ACCORDANCE WITH DRIVEWAY WIDTH APPROVED BY THE TOWN.
 - IF PERPENDICULAR CUT IS WITHIN 12" (INCHES) FROM A JOINT, THEN THE PARALLEL CUT SHALL BE MADE TO THAT JOINT.

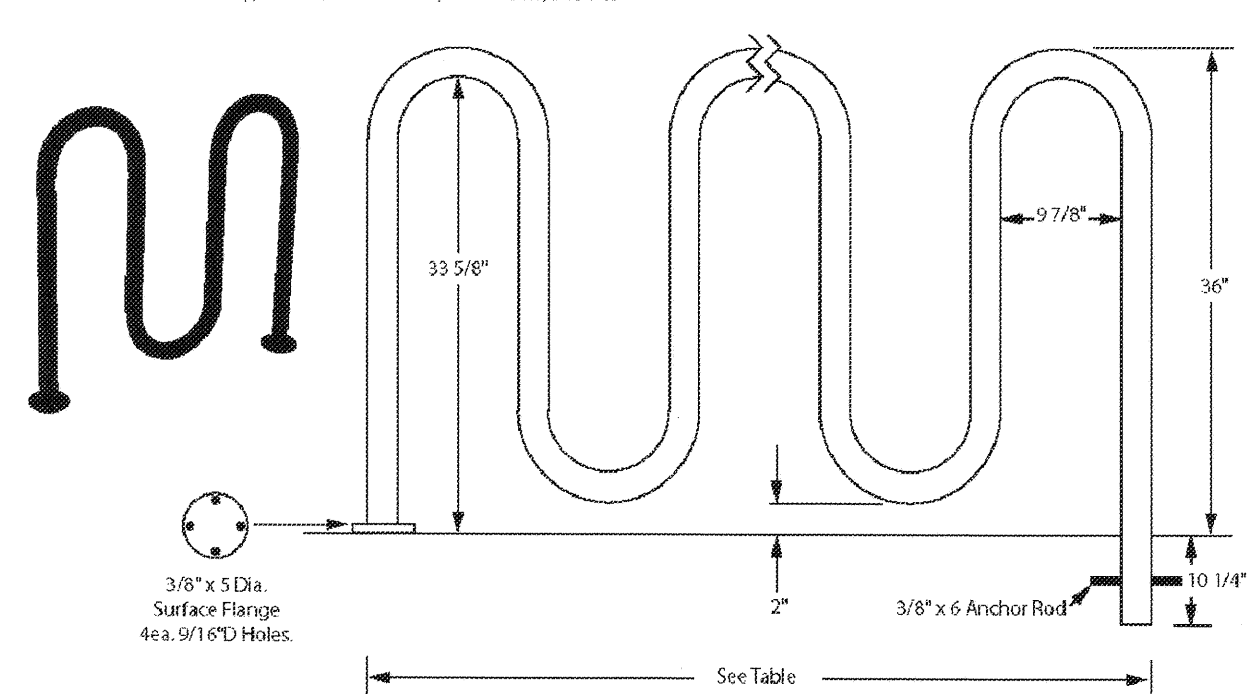
STANDARD METHOD OF REMOVING BACK OF CURB



HANDICAP SIGN N.T.S.

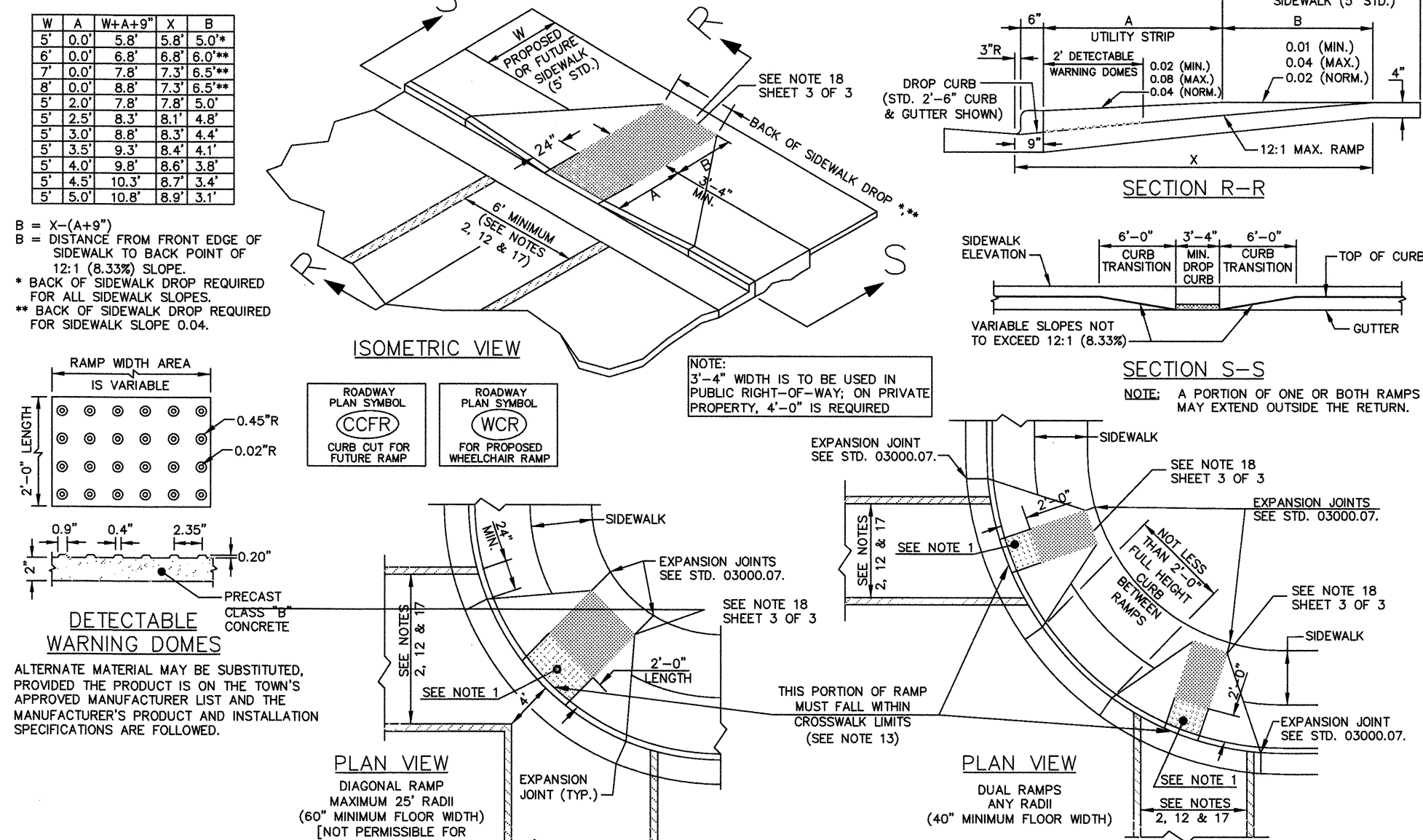
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 Tel: 415.333.6228 Fax: 888.744.2453 Fax: 415.333.2092
 Palmer Group 10121 Canyon Street #105 San Francisco, CA 94623

Welle Rack™
 Multiple Bend

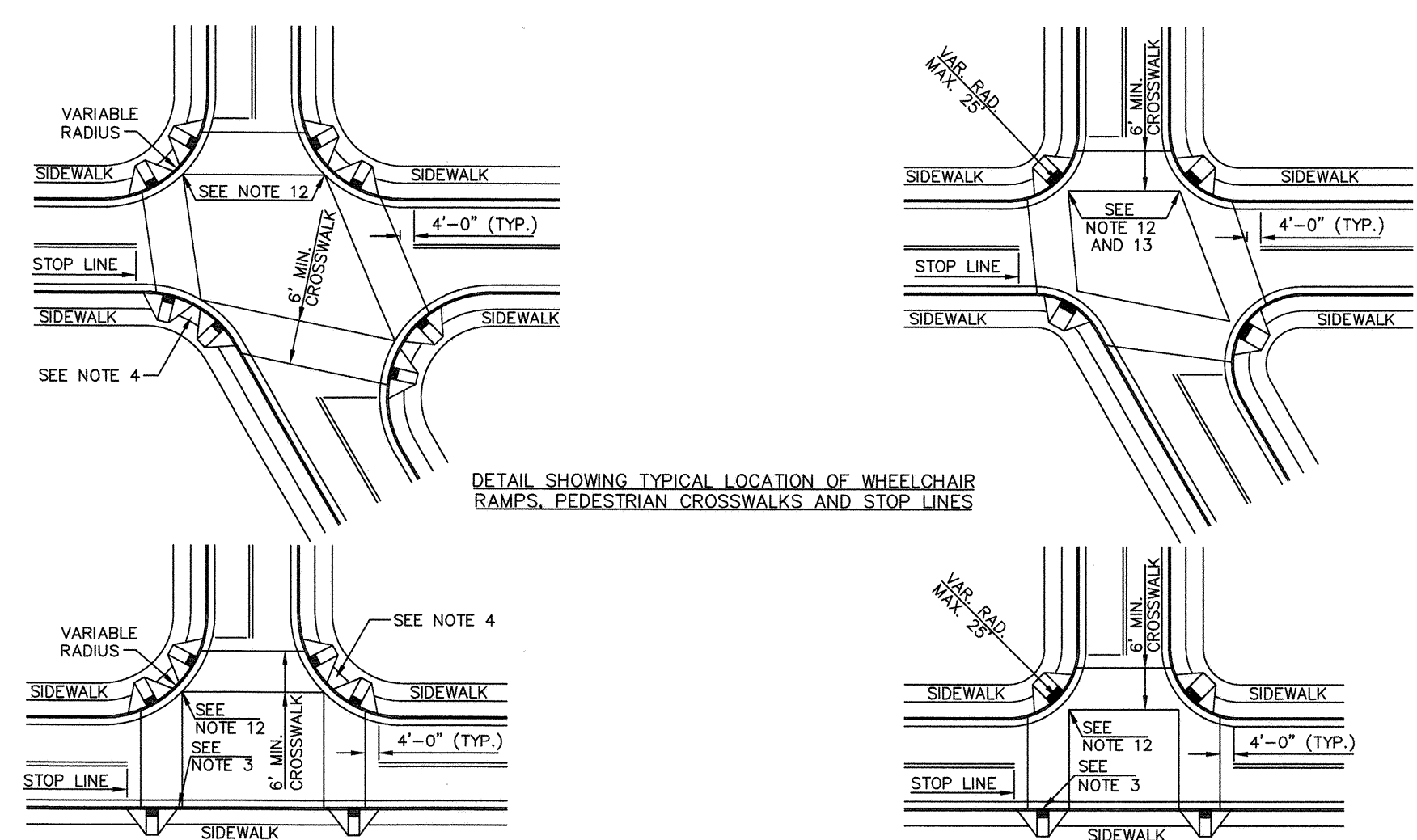


WELLE RACKS					
2-3/8"OD 36TH Multi-Bend Bike Rack					
Item	Finish/Mount	# of Loops	Bike Capacity	Rack Length	Model
H106-5	-G (Common)	3	5	38"	55"
H106-7	-G (Common)	5	7	62"	97"
H106-9	-G (Common)	7	9	86"	135"
H106-11	-SS (Stainless Steel)	9	11	110"	173"
H106-13	-SP (Clearance Flange)	11	13	134"	211"
H106-15	-G (Common)	13	15	158"	249"
H106-17	-G (Common)	15	17	182"	287"

BIKE RACK DETAIL NOTE: BIKE RACK TO BE MANUFACTURER'S STANDARD SILVER METALLIC COLOR



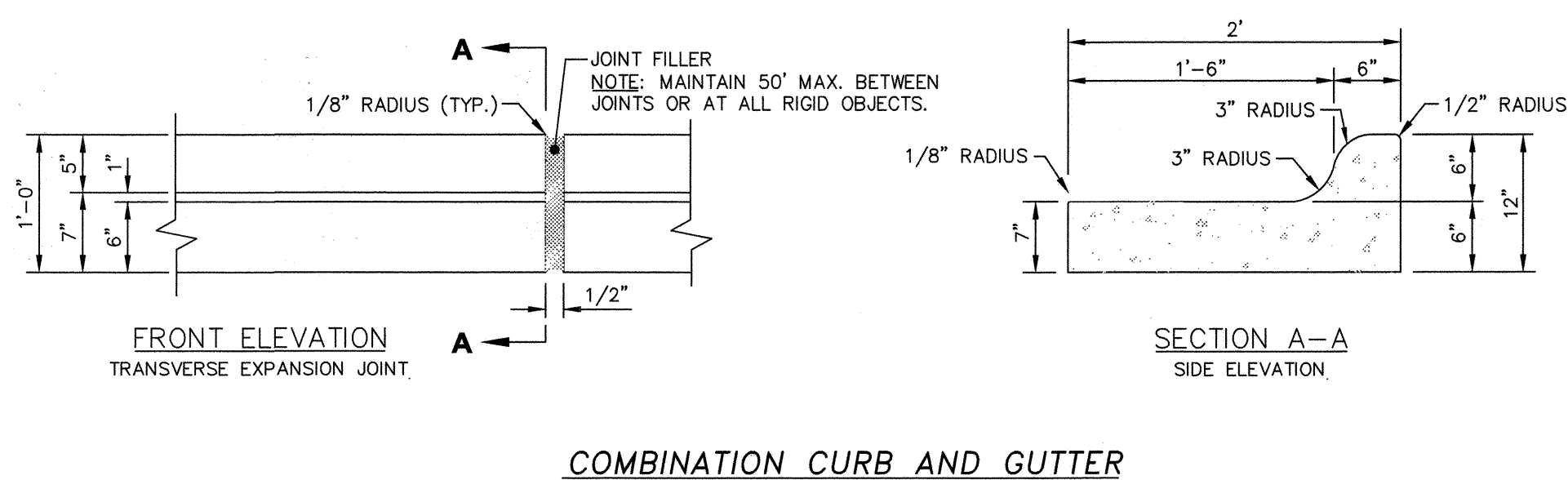
STANDARD WHEELCHAIR RAMP



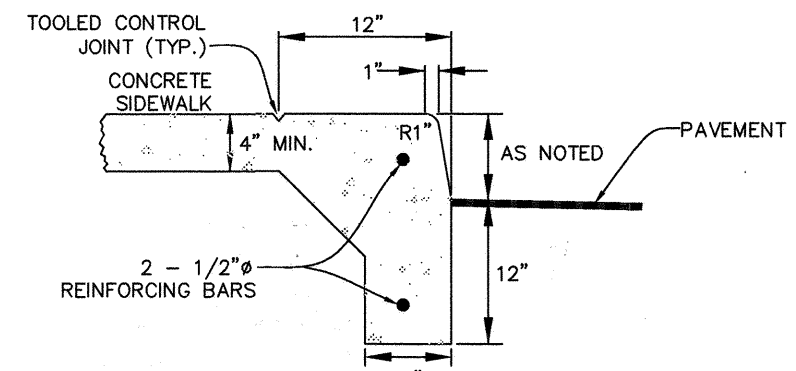
STANDARD WHEELCHAIR RAMP

- NOTES:
- DETECTABLE WARNING DOMES SHALL COVER 2'-0" IN LENGTH AND FULL WIDTH OF THE RAMP FLOOR AS SHOWN ON DETAIL 03000.14, SHEETS 1 & 2. THE RAMP MAY BE YELLOW IN COLOR OR ANY COLOR WITH A 70% CONTRAST RATIO.
 - CROSSWALK WIDTHS AND CONFIGURATION VARY BUT MUST CONFORM TO TRAFFIC DESIGN STANDARDS.
 - NORTH CAROLINA GENERAL STATUTE 136-44.14 REQUIRES THAT ALL STREET CURBS BEING CONSTRUCTED OR RECONSTRUCTED FOR MAINTENANCE PROCEDURES, TRAFFIC OPERATIONS, REPAIRS, CORRECTION OF UTILITIES OR ALTERED FOR ANY REASON AFTER SEPTEMBER 1, 1973 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICALLY DISABLED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER POINTS OF PEDESTRIAN FLOW.
 - IN ADDITION, SECTION 229 OF THE 1973 FEDERAL AND HIGHWAY SAFETY ACT REQUIRES PROVISION OF CURB RAMPS ON ANY CURB CONSTRUCTION AFTER JULY 1, 1976 WHETHER A SIDEWALK IS PROPOSED INITIALLY OR IS PLANNED FOR A FUTURE DATE.
 - THE AMERICANS WITH DISABILITIES ACT (ADA) OF 1990 EXTENDS TO INDIVIDUALS WITH DISABILITIES. COMPREHENSIVE CIVIL RIGHTS PROTECTIONS SIMILAR TO THOSE PROVIDED TO PERSONS ON THE BASIS OF RACE, SEX, NATIONAL ORIGIN AND RELIGION UNDER THE CIVIL RIGHTS ACT OF 1964. THESE CURB RAMPS HAVE BEEN DESIGNED TO COMPLY WITH THE CURRENT ADA STANDARDS.
 - PROVIDE WHEELCHAIR RAMPS AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. LOCATE WHEELCHAIR RAMPS AS DIRECTED BY THE ENGINEER WHERE EXISTING LIGHT POLES, FIRE HYDRANTS, DROP INLETS, ETC. AFFECT PLACEMENT. WHERE TWO RAMPS ARE INSTALLED PLACE NOT LESS THAN 2 FEET OF FULL HEIGHT CURB BETWEEN THE RAMPS. PLACE DUAL RAMPS AS NEAR PERPENDICULAR TO THE TRAVEL LANE BEING CROSSED AS POSSIBLE.
 - PAY FOR ALL VARIABLE DEPTH CONCRETE USED FOR CONSTRUCTION OF WHEELCHAIR RAMPS AS CONCRETE WHEELCHAIR RAMPS. (SQ. YDS.)
 - PAY FOR ALL DEPRESSED CURBS AT WHEELCHAIR RAMPS AS THE TYPE CURB AND GUTTER USED ADJACENT TO DEPRESSED CURB. (LN. FT.)
 - SUCH PRICES AND PAYMENTS IS CONSIDERED FULL COMPENSATION FOR ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO SATISFACTORILY COMPLETE THE WORK.
 - DO NOT EXCEED 0.08 (12:1) SLOPE ON THE WHEELCHAIR RAMP IN RELATIONSHIP TO THE GRADE OF THE STREET.
 - CONSTRUCT WHEELCHAIR RAMPS 40" (3'-4") OR GREATER FOR DUAL RAMPS AND 60" (5'-0") OR GREATER FOR DIAGONAL RAMPS.
 - USE CLASS "B" CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SKID TYPE SURFACE.
 - PLACE A 1/2" EXPANSION JOINT WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB AND AS SHOWN ON STANDARD DETAIL 03000.07.
 - PLACE THE INSIDE PEDESTRIAN CROSSWALK LINES NO CLOSER IN THE INTERSECTION WITH ALLOWANCE OF A 4' CLEAR ZONE IN THE VEHICULAR TRAVELWAY WHEN ONE RAMP IS INSTALLED. (SEE NOTE 17)
 - COORDINATE THE CURB CUT AND THE PEDESTRIAN CROSSWALK LINES SO THE FLOOR OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES. PLACE DIAGONAL RAMPS WITH FLARED SIDES SO 24" OF FULL HEIGHT CURB FALLS WITHIN THE CROSSWALK MARKINGS ON EACH SIDE OF THE FLARES.
 - CONSTRUCT THE PEDESTRIAN CROSSWALK A MINIMUM OF 8 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRABLE.
 - USE STOP LINES, NORMALLY PERPENDICULAR TO THE LANE LINES, WHERE IT IS IMPORTANT TO INDICATE THE POINT BEHIND WHICH VEHICLES ARE REQUIRED TO STOP IN COMPLIANCE WITH A TRAFFIC SIGNAL, STOP SIGN OR OTHER LEGAL REQUIREMENT. AN UNUSUAL APPROACH SKEW MAY REQUIRE THE PLACEMENT OF THE STOP LINE TO BE PARALLEL TO THE INTERSECTING ROADWAY.
 - TERMINATE PARKING A MINIMUM OF 20 FEET BACK OF PEDESTRIAN CROSSWALK.
 - PLACE ALL PAVEMENT MARKINGS IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION AND THE NORTH CAROLINA SUPPLEMENT TO THE MUTCD.
 - FROM DETECTABLE WARNING DOME PORTION ALL THE WAY TO BACK EDGE OF SIDEWALK NEEDS TO BE BLACK.

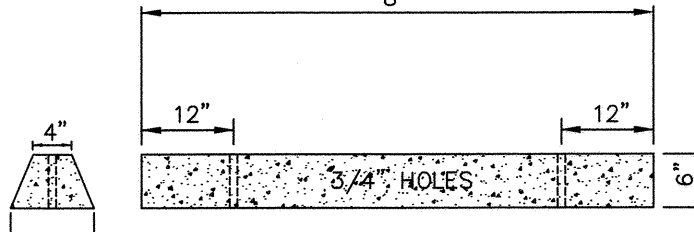
STANDARD WHEELCHAIR RAMP



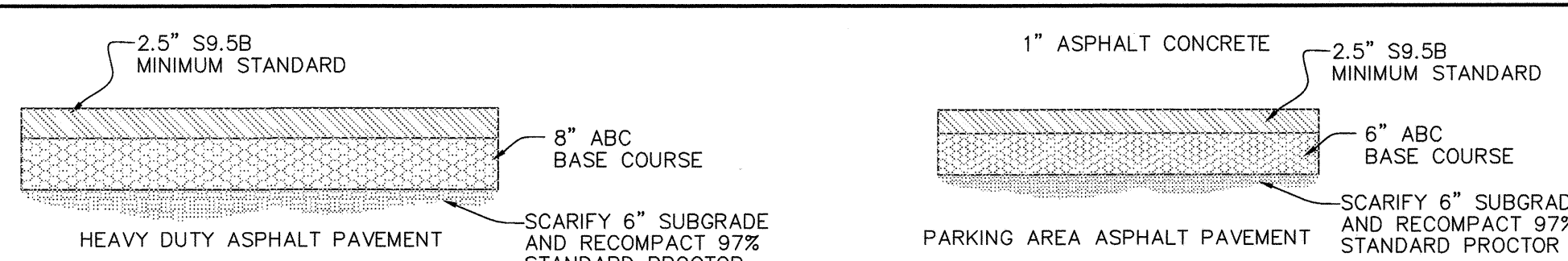
COMBINATION CURB AND GUTTER



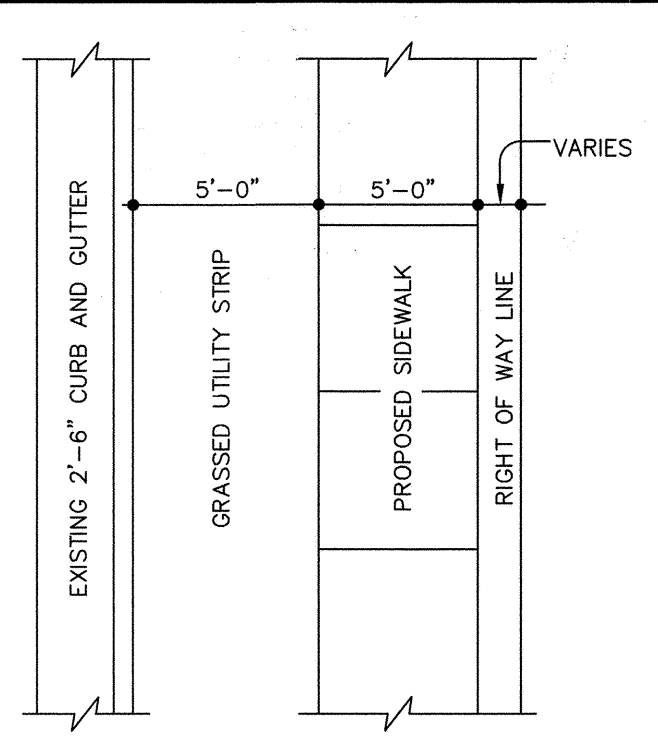
INTEGRAL SIDEWALK AND CURB



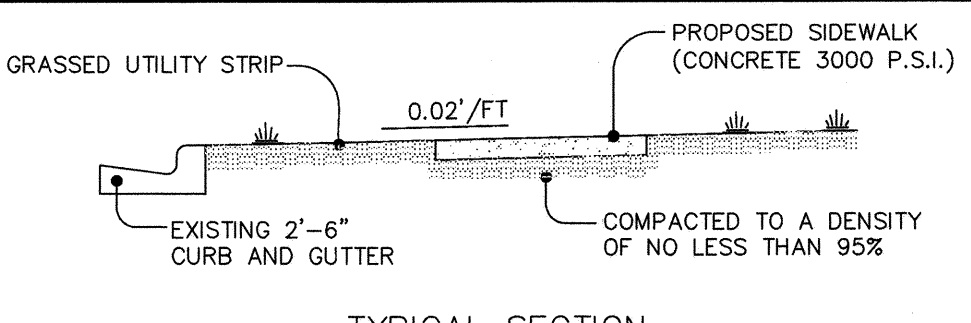
CONC. BUMPER BLOCK



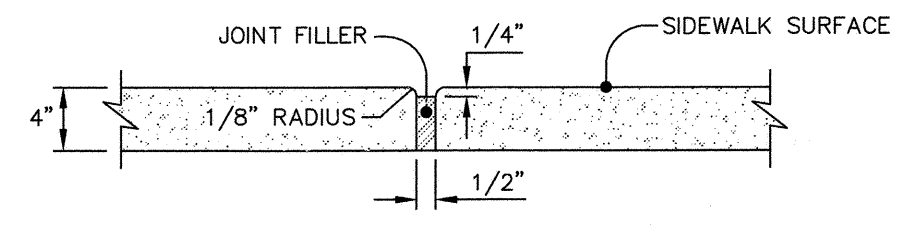
ASPHALT PAVEMENT DETAILS



STANDARD CONCRETE SIDEWALK

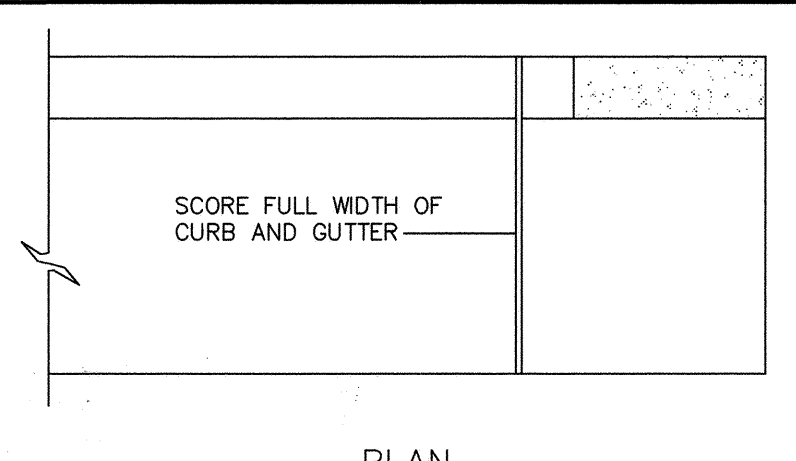


TYPICAL SECTION

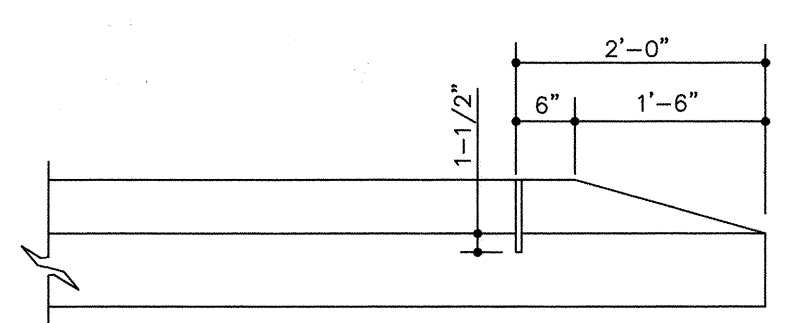


TRANSVERSE EXPANSION JOINT

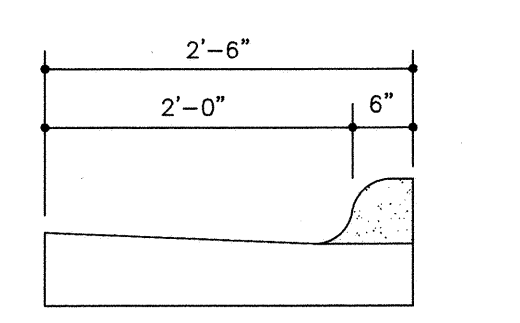
- NOTES:
- TRANSVERSE EXPANSION JOINTS TO BE A MAXIMUM OF 50 FEET.
 - ALL CONCRETE TO BE FINISHED WITH CURING COMPOUND.
 - A 6-INCH DEPTH IS REQUIRED AT LOCATIONS OF DRIVEWAY CROSSINGS AND STREET INTERSECTIONS.



PLAN

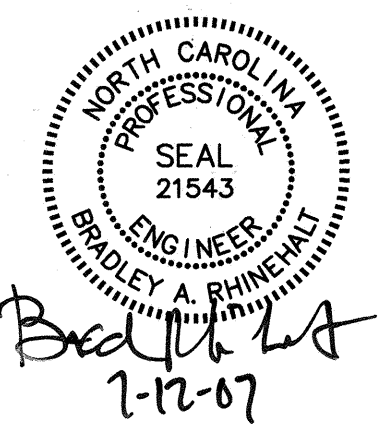


FRONT



END

STANDARD METHOD OF ENDING CURB AND GUTTER



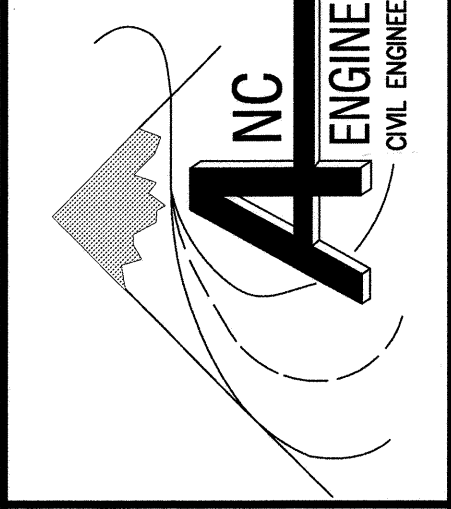
NOTE:
 1. ALL CONSTRUCTION WILL CONFORM TO TOWN OF CARY STANDARDS, DETAILS, AND SPECIFICATIONS.

07-SP-018
 APPROVED
 TOWN OF CARY
 Approved by *DEC* Date *7/19/07*
 Planning *KAH* Date *7/12/07*
 Engineering *TS* Date *7-13-07*

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TOWN OF CARY
 WAKE COUNTY
 NORTH CAROLINA

CLIENT:
 REVISED

PER TOWN OF CARY 07-12-07
 PER TOWN OF CARY 06-07-07
 PER TOWN OF CARY 04-26-07

DATE: 03-08-07
 DRAWN BY: TRS
 CHECKED BY: BAR

SCALE: HOR 1" = 10' FT.
 VER 1" = 10' FT.
 9
 07-517