

REVISIONS	
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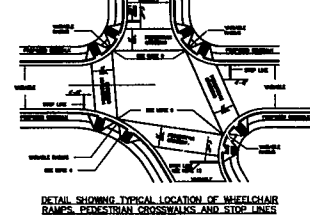
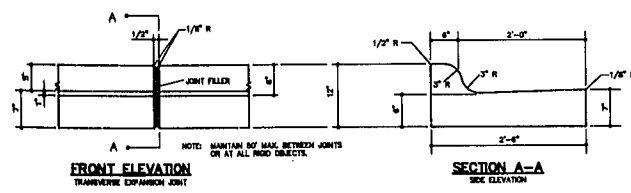
SHEET 1 OF 2  
STD. No.

A.B.C. UNDER 2'-6" CURB & GUTTER

STD. No. 3.02

STANDARD WHEELCHAIR RAMP

STD. No. 3.16



- NOTES:**
1. THE RAMP SURFACE SHALL BE SLIP RESISTANT.
  2. THE RAMP SURFACE SHALL BE FINISHED WITH A FINISH THAT PROVIDES A COEFFICIENT OF FRICTION OF AT LEAST 0.45. THE RAMP SHALL BE FINISHED WITH A FINISH THAT PROVIDES A COEFFICIENT OF FRICTION OF AT LEAST 0.45 AND CONFORMS TO THE REQUIREMENTS OF FEDERAL AASHTO. THE RAMP SHALL BE FINISHED WITH A FINISH THAT PROVIDES A COEFFICIENT OF FRICTION OF AT LEAST 0.45 AND CONFORMS TO THE REQUIREMENTS OF FEDERAL AASHTO.
  3. NORTH CAROLINA GENERAL STATUTE 136-4414 REQUIRES THAT ALL STREET CURBS BE CONSTRUCTED OR RECONSTRUCTED FOR HANDICAPPED PERSONNEL. THESE PROVISIONS REQUIRE CONSTRUCTION OF CURBS OR ALTERNATE FOR ANY REASON AFTER SEPTEMBER 1, 1975 SHALL PROVIDE WHEELCHAIR RAMPS FOR THE PHYSICAL HANDICAPPED AT ALL INTERSECTIONS WHERE BOTH CURB AND GUTTER AND SIDEWALKS ARE PROVIDED AND AT OTHER LOCATIONS WHERE THE PHYSICAL HANDICAPPED ARE REQUIRED TO CROSS A STREET. THE RAMP SHALL BE FINISHED WITH A FINISH THAT PROVIDES A COEFFICIENT OF FRICTION OF AT LEAST 0.45 AND CONFORMS TO THE REQUIREMENTS OF FEDERAL AASHTO.
  4. WHEELCHAIR RAMPS SHALL BE PROVIDED AT LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. WHEELCHAIR RAMPS SHALL BE LOCATED AS SHOWN OR AS DIRECTED BY THESE DETAILS. THE LOCATION MAY BE CHANGED AS DIRECTED BY THE ENGINEER. WHEELCHAIR RAMPS SHALL BE FINISHED WITH A FINISH THAT PROVIDES A COEFFICIENT OF FRICTION OF AT LEAST 0.45 AND CONFORMS TO THE REQUIREMENTS OF FEDERAL AASHTO.
  5. NO SLOPE ON THE WHEELCHAIR RAMP SHALL EXCEED 1" (1:12) IN RELATIONSHIP TO GRADE OF THE STREET.
  6. IN NO CASE SHALL THE WIDTH OF THE WHEELCHAIR RAMP BE LESS THAN 4' (3'-0" MINIMUM WIDTH MAY BE USED).
  7. USE CLASS 'F' CONCRETE WITH A SIDEWALK FINISH IN ORDER TO OBTAIN A ROUGH NON-SLIP TYPE SURFACE. (SEE SHEET 1 FOR SURFACE FINISH).
  8. A 1/2" EXPANSION JOINT WILL BE REQUIRED WHERE THE CONCRETE WHEELCHAIR RAMP JOINS THE CURB.
  9. THE WHEELCHAIR RAMP SHALL BE FINISHED WITH A FINISH THAT PROVIDES A COEFFICIENT OF FRICTION OF AT LEAST 0.45 AND CONFORMS TO THE REQUIREMENTS OF FEDERAL AASHTO.
  10. THE CURB OUT AND THE PEDESTRIAN CROSSWALK LINES SHALL BE CONFORMED TO THAT THE BOUNDARY OF THE WHEELCHAIR RAMP WILL FALL WITHIN THE PEDESTRIAN CROSSWALK LINES.
  11. THE WIDTH OF THE PEDESTRIAN CROSSWALK SHALL BE 6 FEET. A CROSSWALK WIDTH OF 10 FEET OR GREATER IS DESIRED.
  12. STOP LINES, NORMALLY PERPENDICULAR TO THE LINE LINES, SHALL BE USED WHERE IT IS IMPRACTICAL TO LOCATE THE STOP LINES PARALLEL TO THE WALKWAY. STOP LINES SHALL BE LOCATED SUCH THAT THE FACE OF THE STOP LINE IS PARALLEL TO THE WALKWAY.
  13. PAVING SHALL BE SUBMITTED A MINIMUM OF 30 FEET BACK OF PEDESTRIAN CROSSWALK.
  14. ALL PAVING MATERIALS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM PRACTICE CONTROLLED SPECIFICATIONS FOR THE MATERIALS OF HIGHWAY CONSTRUCTION AND THE NORTH CAROLINA SUPPLEMENT TO THE SPECIFICATIONS.

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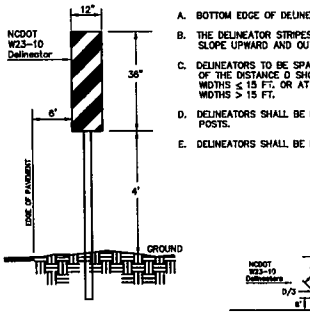
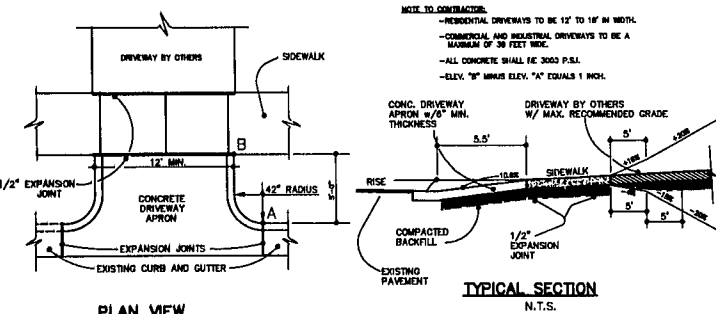
SHEET 2 OF 2  
STD. No.

STANDARD CONCRETE CURB AND GUTTER

STD. No. 3.11

STANDARD WHEELCHAIR RAMP

STD. No. 3.16



- NOTES:**
- A. BOTTOM EDGE OF DELINEATOR 4' ABOVE ROADWAY.
  - B. THE DELINEATOR STRIPES SHALL SLOPE UPWARD SLOPE UPWARD AND OUTWARD FROM TRAFFIC.
  - C. DELINEATORS TO BE SPACED ON CENTERS AT 1/3 OF THE DISTANCE D SHOWN BELOW FOR NEW ASPHALT WIDTHS < 16 FT. OR AT 1/4 OF D FOR NEW ASPHALT WIDTHS > 16 FT.
  - D. DELINEATORS SHALL BE MOUNTED ON BREAKAWAY POSTS.
  - E. DELINEATORS SHALL BE REFLECTORIZED.
1. TAPER ON BOTH ENDS OF ROADWAY WIDENING SHALL BE A MINIMUM 2:1. THE TOWN ENGINEER AND/OR NOTION RESERVE THE RIGHT TO REQUIRE A LONGER TAPER IF DEEMED NECESSARY FOR THE SAFETY OF THE PUBLIC.
  2. A SOLID WHITE EDGE MARKING SHALL BE EXTENDED ALONG WIDENING AT EXISTING PAVEMENT.
  3. DELINEATORS SHALL ONLY BE REQUIRED AT TAPER FROM CURB TO EXISTING PAVEMENT IN DIRECTION OF TRAVEL.
  4. DELINEATORS SHALL BE ORIENTED SUCH THAT THE FACE OF THE SIGN IS PERPENDICULAR TO TRAVEL LANE.

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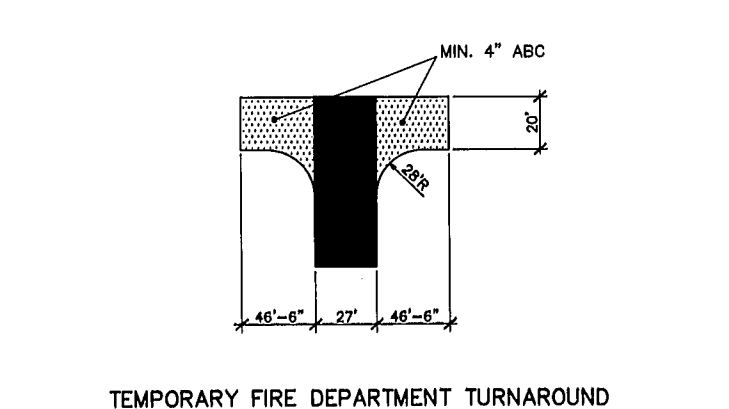
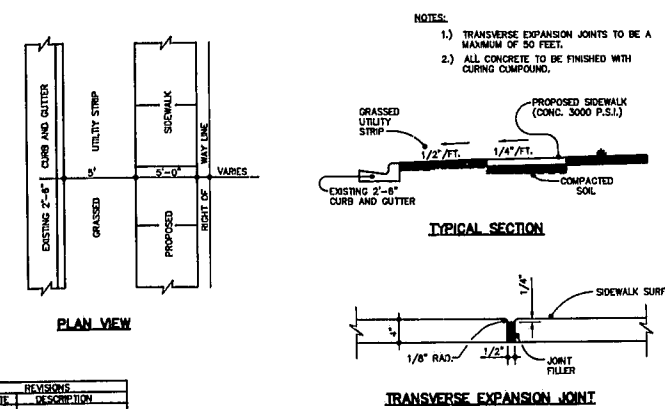
SHEET 2 OF 2  
STD. No.

STANDARD DRIVEWAY APRON

STD. No. 3.15

STANDARD PAVEMENT WIDENING TAPER AND MARKINGS

STD. No. 3.17



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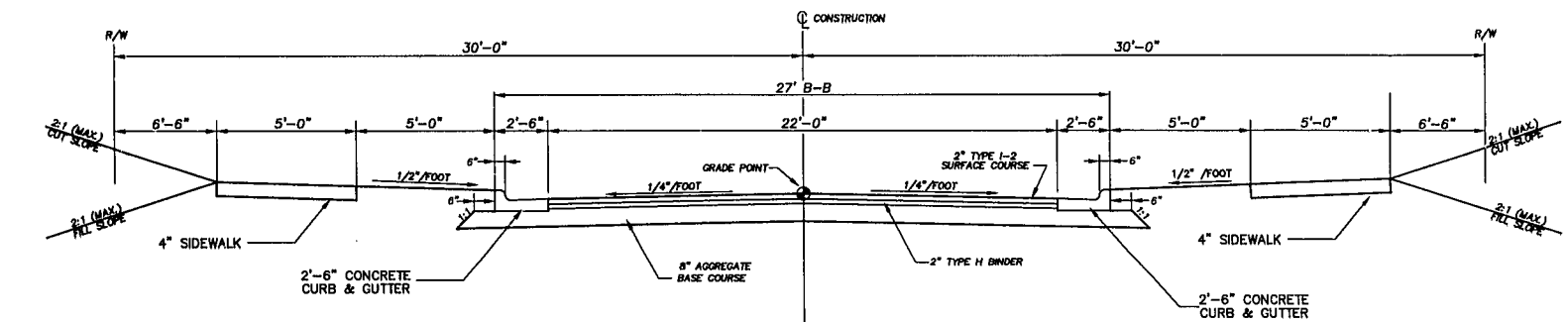
REVISIONS	
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SHEET 2 OF 2  
STD. No.

STANDARD CONCRETE SIDEWALK

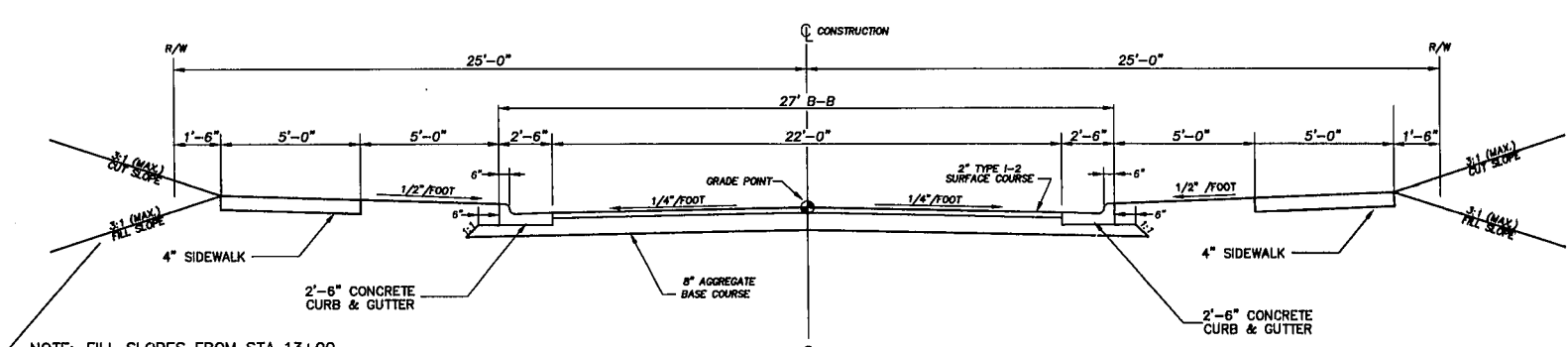
STD. No. 3.09

TEMPORARY FIRE DEPARTMENT TURNAROUND



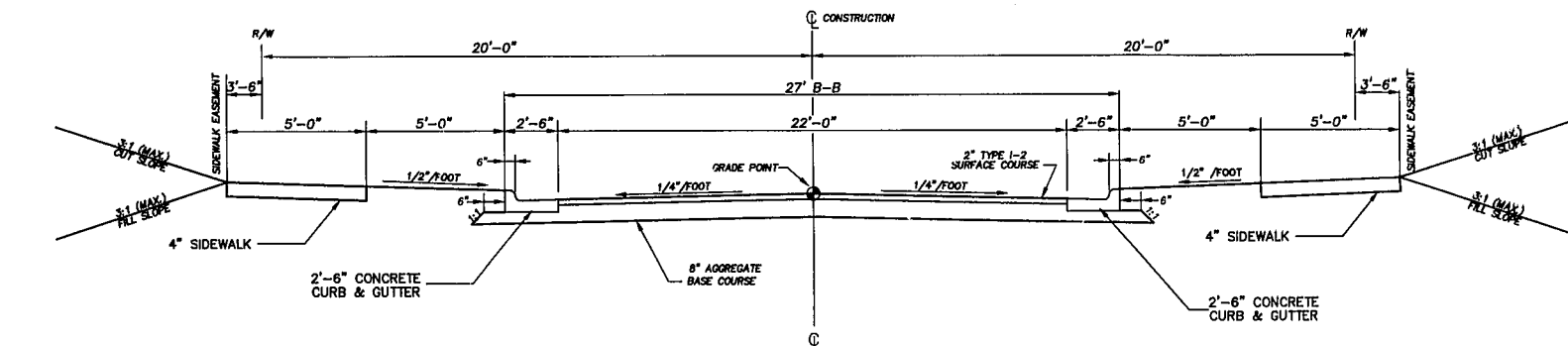
TYPICAL ROADWAY SECTION

CARY GLEN BOULEVARD



TYPICAL ROADWAY SECTION

LANTERN RIDGE LANE (R001), KENNONDALE COURT (R002), LYNDON COURT (R003), TROYCOTT PLACE (R004)



TYPICAL ROADWAY SECTION

HOLLOWRIDGE COURT (R006)

- NOTES:**
1. THE OWNER MAY OPT TO LEAVE THE STREETS ONE INCH LOW UNTIL THE INFRASTRUCTURE IN THE SUBDIVISION IS COMPLETE. THIS OPTION WOULD REQUIRE THAT THE APPROVED PAVEMENT DESIGN BE INCREASED BY ONE-HALF INCH OF ASPHALT.
  2. PAVEMENT DESIGN SHALL BE SUBMITTED TO THE TOWN OF CARY FOR REVIEW AND APPROVAL PRIOR TO PLACEMENT OF ANY STONE BASE.

00-65-008  
APPROVED  
BIB 4/2/01  
TM 4-3-01



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Revisions			
No.	Description	Date	By
A	TOC 1st REVIEW COMMENTS	12/1/00	GAS
B	TOC 2nd REVIEW COMMENTS	1/12/01	GAS
C	TOC FINAL APPROVAL	3/22/01	GAS

CARY PARK P.U.D.  
PARCELS SF-7 AND SF-8b

ROADWAY DETAILS

Designer	W&R	Scale	NTS	CAD File	SHT11.DWG
Drawn By	GAS	Date	10/27/00	Sheet No.	
Checked By	JEC	Job No.	00268		13 of 13